

Boise South Fork Watershed Collaborative

Roads & Trails Committee *Potential Slides for Discussion*

Roads & Trails Committee

- Introductions & Overview
- Mission Statement
- Identify issues
- Inventory of available data
- Identify data gaps/needs
- Identify tool boxes for potential solutions/improvements
- Individual focus areas for participants/agencies
- What's next...

Roads & Trails Committee – Overview

- How do Roads & Trails affect water quality and quantity in the South Fork Watershed?
- What can be done with/to watershed area Roads & Trails to “improve” water quality and quantity?

Roads & Trails

How do Roads & Trails affect water quality and quantity

- At-grade stream crossings — *Trails*
- Drainage flows/erosion: run-off/gradient/slope issues — *Roads & Trails*
- Structures (bridges, culverts, boat ramps) — *Roads*
- Diversions — *Roads & Trails*
- Dust/particulates — *Roads & Trails*
- Loss of native riparian or upland vegetation — *Roads & Trails*
- Introduction of invasive species — *Roads & Trails*
- Others?

Roads & Trails

What aspects of “use” contribute to these effects?

- Volume/usage levels
- Speed/duration
- Mode/vehicle type
- Seasonal impacts/precipitation
- Diversions/trail braiding (including road departures/crashes)
- Waste/trash/debris
- ?

Roads & Trails

How to measure/quantify contributing effects

Roads

- Roadside drainage assessments
- Traffic volumes
- Speed & vehicle-type surveys
- Crash data records
- Bridge & culvert assessments
- Maintenance & repair histories
- User surveys

Trails

- Stream crossing inventories
- Trail conditions assessments
- Usage-type/volumes reports
- Maintenance & repair histories
- Trail-adjacent vegetation assessments
- User surveys

Roads & Trails

- What can be done with/to Roads & Trails to “improve” water quality and quantity?
- What tool boxes are available?
- Short-term vs long-term solutions

Other slides

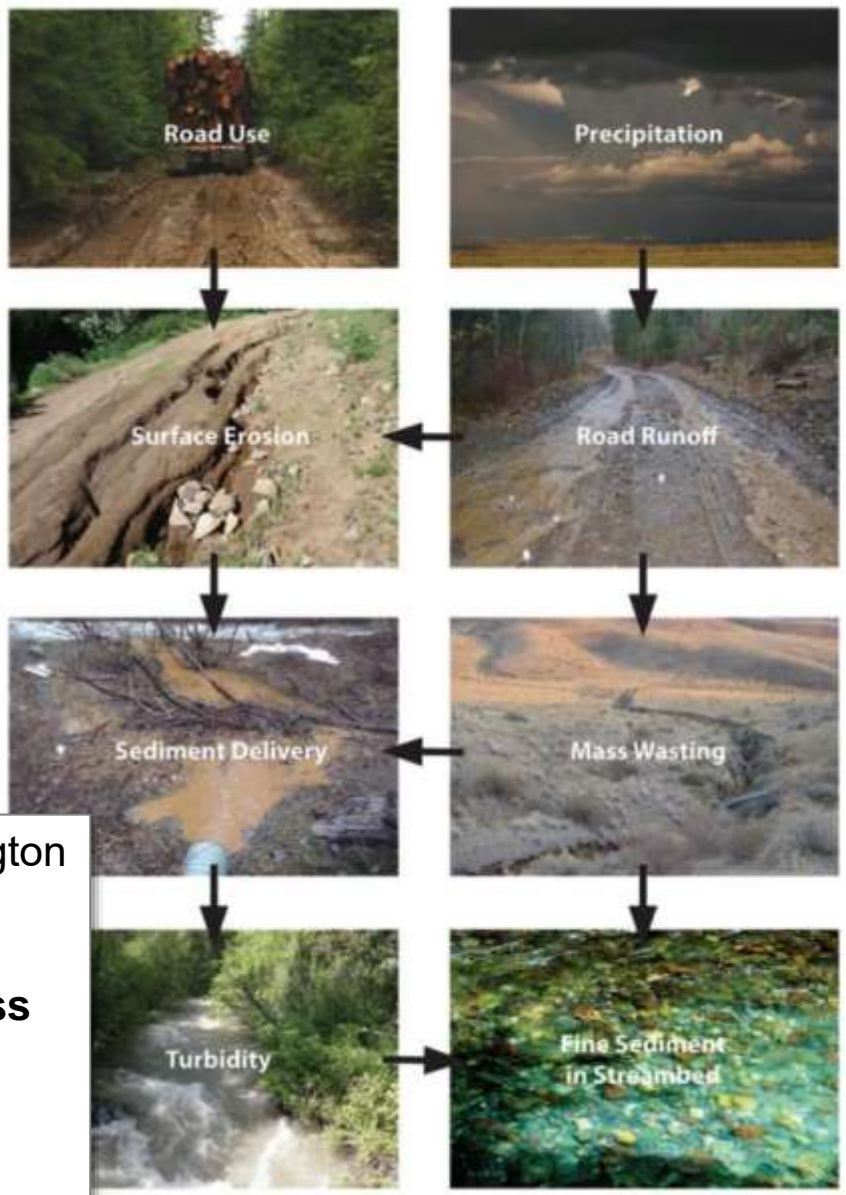
- *Watershed-area Roads & Trails*
- *Motor vehicle crash data*
- *Traffic volume data*
- *Bridge condition reports*

Linkages between unpaved forest roads and streambed sediment: why context matters in directing road restoration

Robert Al-Chokhachy^{1,2}, Tom A. Black³, Cameron Thomas⁴, Charles H. Luce³, Bruce Rieman⁵, Richard Cissel³, Anne Carlson⁶, Shane Hendrickson⁷, Eric K. Archer⁸, Jeff L. Kershner¹

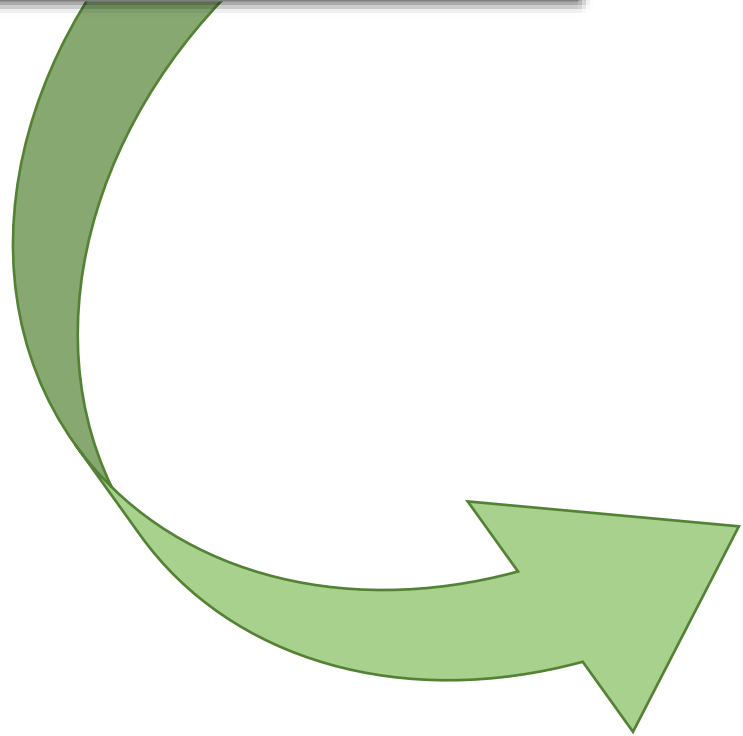
Unpaved forest roads remain a pervasive disturbance on public lands and mitigating sediment from road networks remains a priority for management agencies. Restoring roaded landscapes is becoming increasingly important for many native cold-water fishes that disproportionately rely on public lands for persistence.

Unpaved forest roads remain a pervasive disturbance on public lands and mitigating sediment from road networks remains a priority for management agencies. Restoring roaded landscapes is becoming increasingly important for many native cold-water fishes that disproportionately rely on public lands for persistence.



“The total extent of unpaved forest roads in Idaho, Montana, Oregon, and Washington alone is **seven times the length of interstate highways** across the contiguous United States. Many of these roads have been identified as **causing significant impacts to streams and riparian systems through changes in hydrology, mass wasting and sediment delivery, and riparian degradation** (Jones et al. 2000; Trombulak & Frissell 2000). Because there are hundreds of thousands of miles of existing unpaved roads, prioritization of restoration actions is an imperative step in effectively recovering ecosystems.

“In some cases, road sediments that are related to culvert failures, landslides, and gullies present obvious locations for road rehabilitation or restoration.”



Prairie Road Sediment Impact Report

South Fork Boise River Basin, Pierce Creek Watershed

Boise National Forest

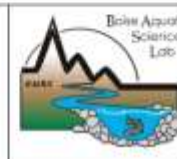


April 2010

Richard Cissel², Tom Black¹, Nathan Nelson², and Chase Fly²

¹Hydrologist
²Hydrologic Technician

US Forest Service
Rocky Mountain Research Station
322 East Front Street, Suite 401
Boise, Idaho, 83702 USA



Roadway Vehicle Crash History

Results for FeatureType 'Ihsip:crash_mv':

```

serial_number = 22C622648
highway_system = local
severity = A Injury Accident
units = 1
accident_year = 2022
accident_date = 2022-12-19
accident_time = 14:48
day_of_week = Monday
intersection_related = false
street1 = Prairie Rd
street2 = null
reference_street = Joy Ln
dist_from_intersection = 1863,000 ft N
intersection_type = Not at intersection
road_type = 2-Way & No Divider
speedlimit_street1 = 45
speedlimit_street2 = null
direction_of_travel = S
driver_action = Negotiating Curve
vision_obstruction = None
impaired = false
lane_dep = false
first_harmful_event = Overturn
most_harmful_event = Overturn
events = Loss of Control,Ran Off Road,Overturn,Vehicle Equipment Failure (Blown Tire/Br
contrib_circ_1 = Speed Too Fast For Conditions
contrib_circ_2 = Brakes
contrib_circ_3 = None
road_surface = Dirt
road_surface_condition = Dry
other_road_conditions = None
weather_condition1 = Clear
weather_condition2 = null
light_condition = Day
traffic_control_device = None
traffic_control_function = null
geometrics_horizontal = Curve
geometrics_vertical = Upgrade or Downgrade
age = 50
state_of_drivers_license = Idaho
latitude = 43.407856887157
longitude = -115.563049771291
local_agency_code = 1230
local_agency_name = Mountain Home HD
road_juris_code = 1230
road_juris_name = Mountain Home HD
crash_mv_id = 107589
the_geom = [GEOMETRY (Point) with 1 points]

```

County

Road Jurisdiction

Ln 50, Col 44 | 100% | Windows (CRLF) | UTF-8

Idaho Crash Data | LHTAC Projects | Idaho Small Structures (10-20

Search by location name or coordinates ...

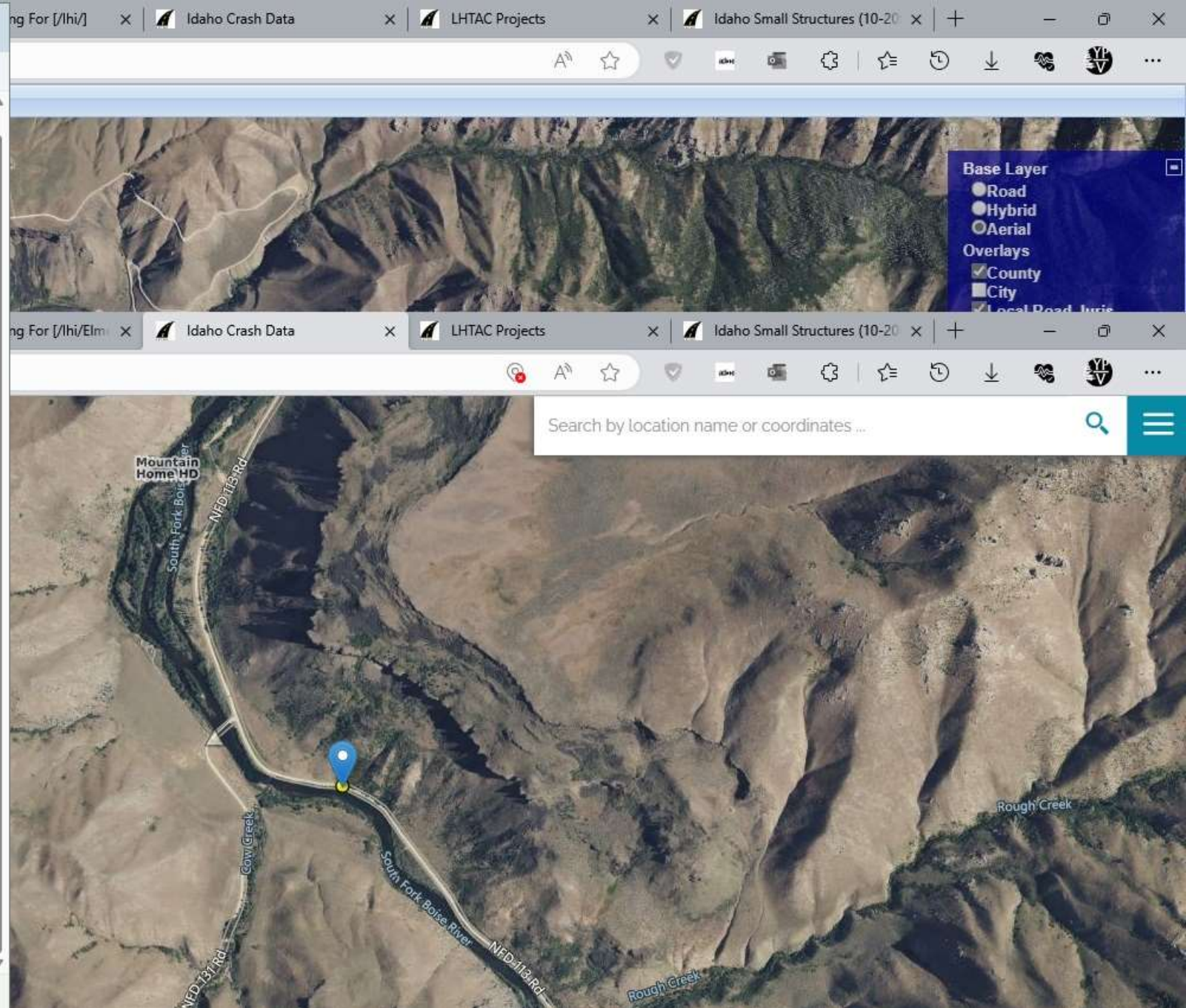
Microsoft Corporation, © 2023 Maxar, ©CNES
 Distribution Airbus DS, © 2023 TomTom, Bing

Results for FeatureType 'Ihsipcrash'

File Edit View

serial_number = 21C576493
 highway_system = local
 severity = C Injury Accident
 units = 1
 accident_year = 2021
 accident_date = 2021-06-29
 accident_time = 16:00
 day_of_week = Tuesday
 intersection_related = false
 street1 = Anderson Ranch Dam Rd
 street2 = null
 reference_street = NF-131 (Cow Creek)
 dist_from_intersection = 0.250 Mile S
 intersection_type = Not at intersection
 road_type = 2-Way & No Divider
 speedlimit_street1 = 25
 speedlimit_street2 = null
 direction_of_travel = N
 driver_action = Going Straight
 vision_obstruction = None
 impaired = false
 lane_dep = true
 first_harmful_event = Building/Wall
 most_harmful_event = Building/Wall
 events = Building/Wall,Ran Off Road,
 contrib_circ_1 = Speed Too Fast For Conditions
 contrib_circ_2 = Failed to Maintain Lane
 contrib_circ_3 = None
 road_surface = Dirt
 road_surface_condition = Dry
 other_road_conditions = None
 weather_condition1 = Clear
 weather_condition2 = null
 light_condition = Dark, No Street Lights
 traffic_control_device = None
 traffic_control_function = null
 geometrics_horizontal = Straight
 geometrics_vertical = Level
 age = 59
 state_of_drivers_license = Idaho
 latitude = 43.364755136484
 longitude = -115.547878171743
 local_agency_code = 1230
 local_agency_name = Mountain Home HD
 road_juris_code = 1230
 road_juris_name = Mountain Home HD
 crash_mv_id = 65015
 the_geom = [GEOMETRY (Point) with 1 points]

Ln 14, Col 39 | 100% | Windows (CRLF) | UTF-8



File Edit View

Results for FeatureType 'lhispcrash_mv':

```

serial_number = 22C604513
highway_system = local
severity = B Injury Accident
units = 1
accident_year = 2022
accident_date = 2022-05-28
accident_time = 22:00
day_of_week = Saturday
intersection_related = false
street1 = Fork Rd
street2 = null
reference_street = Anderson Dam Rd
dist_from_intersection = 5,300 Mile W
intersection_type = Not at intersection
road_type = 2-Way & No Divider
speedlimit_street1 = 35
speedlimit_street2 = null
direction_of_travel = W
driver_action = Negotiating Curve
vision_obstruction = None
impaired = false
lane_dep = false
first_harmful_event = Overturn
most_harmful_event = Overturn
events = Drove Left of Center,Ran Off Road,Overturn,
contrib_circ_1 = Failed to Maintain Lane
contrib_circ_2 = Drove Left of Center
contrib_circ_3 = Speed Too Fast For Conditions
road_surface = Dirt
road_surface_condition = Mud/dirt/gravel
other_road_conditions = Ruts/Bumps/Holes
weather_condition1 = Cloudy
weather_condition2 = Clear
light_condition = Dawn or Dusk
traffic_control_device = None
traffic_control_function = null
geometrics_horizontal = Curve
geometrics_vertical = Level
age = 18
state_of_drivers_license = Idaho
latitude = 43.332943459016
longitude = -115.532148626615
local_agency_code = 1230
local_agency_name = Mountain Home HD
road_juris_code = 1230
road_juris_name = Mountain Home HD
crash_mv_id = 91122
the_geom = [GEOMETRY (Point) with 1 points]

```

Layers

Safety

Crash Location

- Property Dan
- C Injury (local)
- B Injury (local)

Feature

Lat: 43.33

Crash L

geometri

geometri

age = 18

state_of

latitude =

longitude

local_age

local_age

road_juri

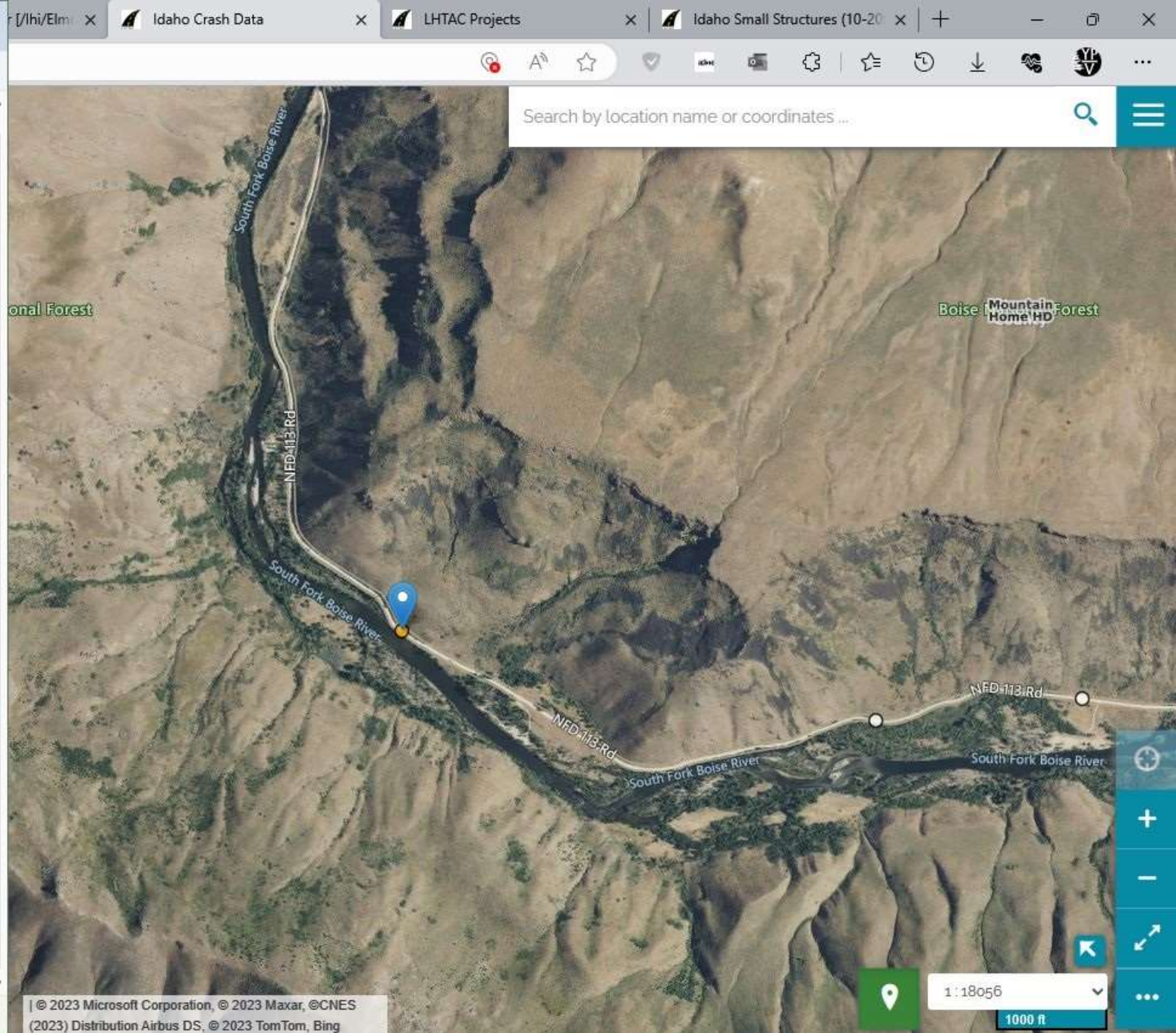
road_juri

crash_mv

the_geom

County

Road Ju



Layers

Safety

Crash Locations

- Property Damage (local)
- C Injury (local)
- B Injury (local)
- A Injury (local)

Feature Info

Lat: 43.33064 - Long: -115.51539

Crash Locations

accident_year = 2019
 accident_date = 2019-10-14
 accident_time = 17:35
 day_of_week = Monday
 intersection_related = false
 street1 = South Fork Rd
 street2 = null
 reference_street = Prairie Rd
 dist_from_intersection = 3.77
 intersection_type = Not at inte
 road_type = 2-Way & No Div
 speedlimit_street1 = 35
 speedlimit_street2 = null

County

Road Jurisdiction

File Edit View

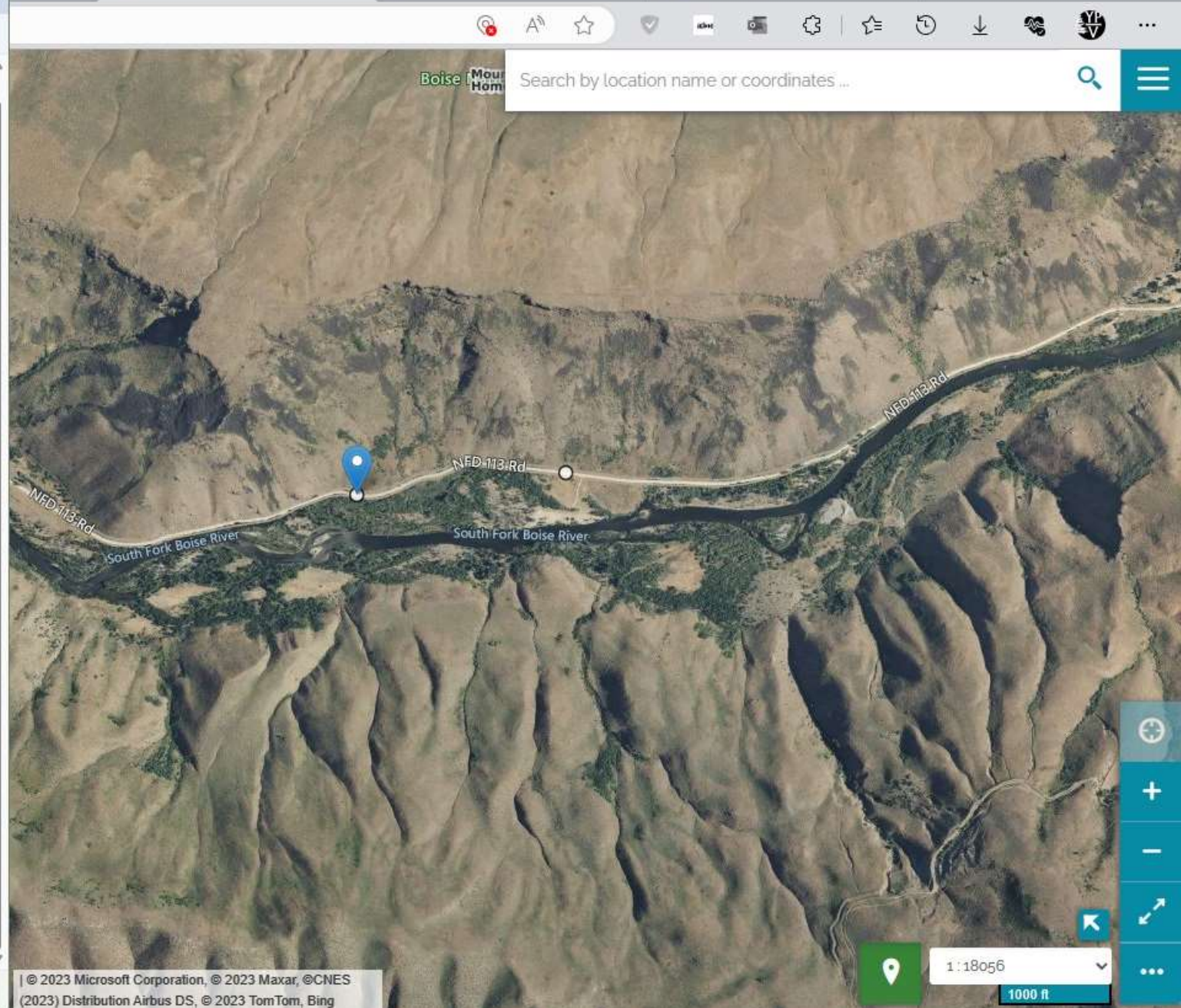
Results for Feature1 type 'insip:crasn_mv':

```

serial_number = 19C533881
highway_system = local
severity = Property Dmg Report
units = 1
accident_year = 2019
accident_date = 2019-10-14
accident_time = 17:35
day_of_week = Monday
intersection_related = false
street1 = South Fork Rd
street2 = null
reference_street = Prairie Rd
dist_from_intersection = 3,770 Mile E
intersection_type = Not at intersection
road_type = 2-Way & No Divider
speedlimit_street1 = 35
speedlimit_street2 = null
direction_of_travel = E
driver_action = Negotiating Curve
vision_obstruction = None
impaired = false
lane_dep = true
first_harmful_event = Embankment
most_harmful_event = Embankment
events = Ran Off Road,Embankment,
contrib_circ_1 = Inattention
contrib_circ_2 = None
contrib_circ_3 = None
road_surface = Dirt
road_surface_condition = Mud/dirt/gravel
other_road_conditions = High/Low Shoulder
weather_condition1 = Clear
weather_condition2 = null
light_condition = Day
traffic_control_device = None
traffic_control_function = null
geometrics_horizontal = Curve
geometrics_vertical = Level
age = 52
state_of_drivers_license = Idaho
latitude = 43.330640858261
longitude = -115.515394605588
local_agency_code = 1230
local_agency_name = Mountain Home HD
road_juris_code = 1230
road_juris_name = Mountain Home HD
crash_mv_id = 27780
the_geom = [GEOMETRY (Point) with 1 points]

```

Ln 50, Col 4 | 100% | Windows (CRLF) | UTF-8



Resources | LHTAC

https://gis.lhtac.org/s

Layers

Safety

Crash Locations

- Property Damage (local)
- C Injury (local)
- B Injury (local)
- A Injury (local)

Feature Info

Lat: 43.3312 - Long: -115.50807

Crash Locations

geometrics_horizontal = Curve
 geometrics_vertical = Level
 age = 44
 state_of_drivers_license = Idaho
 latitude = 43.331205710862
 longitude = -115.508103014484
 local_agency_code = 1230
 local_agency_name = Mountain Home
 road_juris_code = 1230
 road_juris_name = Mountain Home HD
 crash_mv_id = 75219
 the_geom = [GEOMETRY (Point) with 1 points]

County

Road Jurisdiction

Results for Feature1

File Edit View

Results for Feature1 type 'insip:crasn_mv':

```

serial_number = 19C533881
highway_system = local
severity = Property Dmg Report
units = 1
accident_year = 2019
accident_date = 2019-10-14
accident_time = 17:35
day_of_week = Monday
intersection_related = false
street1 = South Fork Rd
street2 = null
reference_street = Prairie Rd
dist_from_intersection = 3.770 Mile E
intersection_type = Not at intersection
road_type = 2-Way & No Divider
speedlimit_streets1 = 35
speedlimit_street2 = null
direction_of_travel = E
driver_action = Negotiating Curve
vision_obstruction = None
impaired = false
lane_dep = true
first_harmful_event = Embankment
most_harmful_event = Embankment
events = Ran Off Road,Embankment,
contrib_circ_1 = Inattention
contrib_circ_2 = None
contrib_circ_3 = None
road_surface = Dirt
road_surface_condition = Mud/dirt/gravel
other_road_conditions = High/Low Shoulder
weather_condition1 = Clear
weather_condition2 = null
light_condition = Day
traffic_control_device = None
traffic_control_function = null
geometrics_horizontal = Curve
geometrics_vertical = Level
age = 52
state_of_drivers_license = Idaho
latitude = 43.330640858261
longitude = -115.515394605588
local_agency_code = 1230
local_agency_name = Mountain Home HD
road_juris_code = 1230
road_juris_name = Mountain Home HD
crash_mv_id = 27780
the_geom = [GEOMETRY (Point) with 1 points]
  
```

Ln 50, Col 4 100% Windows (CRLF) UTF-8

Idaho Crash Data

LHTAC Projects

Idaho Small Structures (10-20

Search by location name or coordinates ...

South Fork Boise River

NFD 113 Rd

Microsoft Corporation, © 2023 Maxar, ©CNES Distribution Airbus DS, © 2023 TomTom, Bing

1:18056

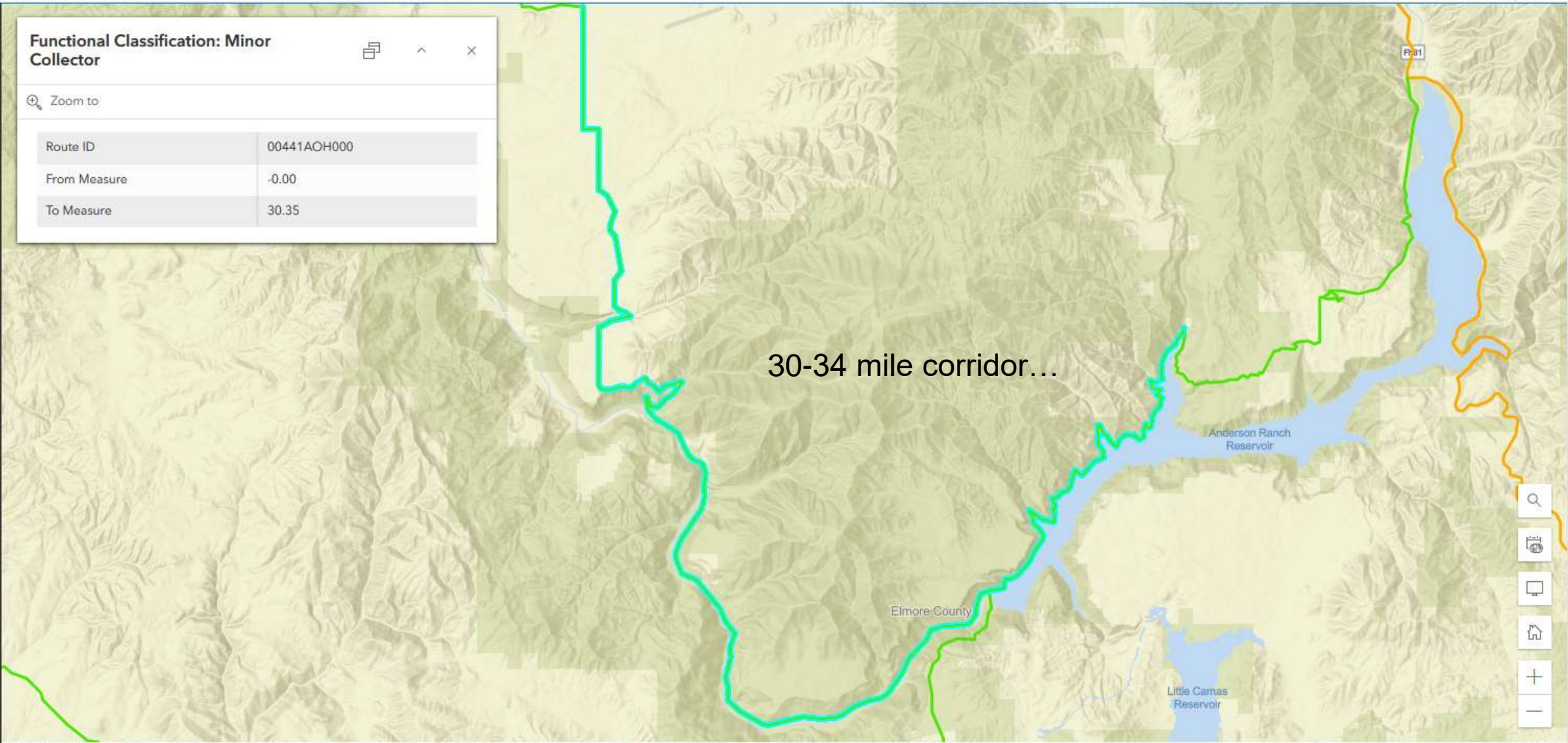
1000 ft

Roadway Traffic Volumes

Functional Classification: Minor Collector

Zoom to

Route ID	00441AOH000
From Measure	-0.00
To Measure	30.35



30-34 mile corridor...

Annual Average Daily Traffic 2022: 00441AOH000

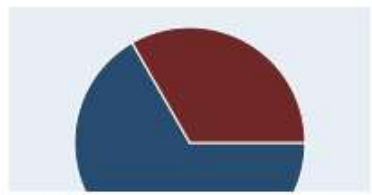
Route ID	00441AOH000
From Measure	3e-8.00000000
To Measure	34.87540066
AADT	30
Passenger AADT	20
Commercial AADT	10
Description From	LONG GULCH RD
Description To	SLOAN GULCH RD
Year	2022
DHV	4
MADT1	11
MADT2	14
MADT3	15
MADT4	14
MADT5	23
MADT6	47
MADT7	67
MADT8	55
MADT9	43
MADT10	32
MADT11	14
MADT12	9

~1 vehicle / 15 mins

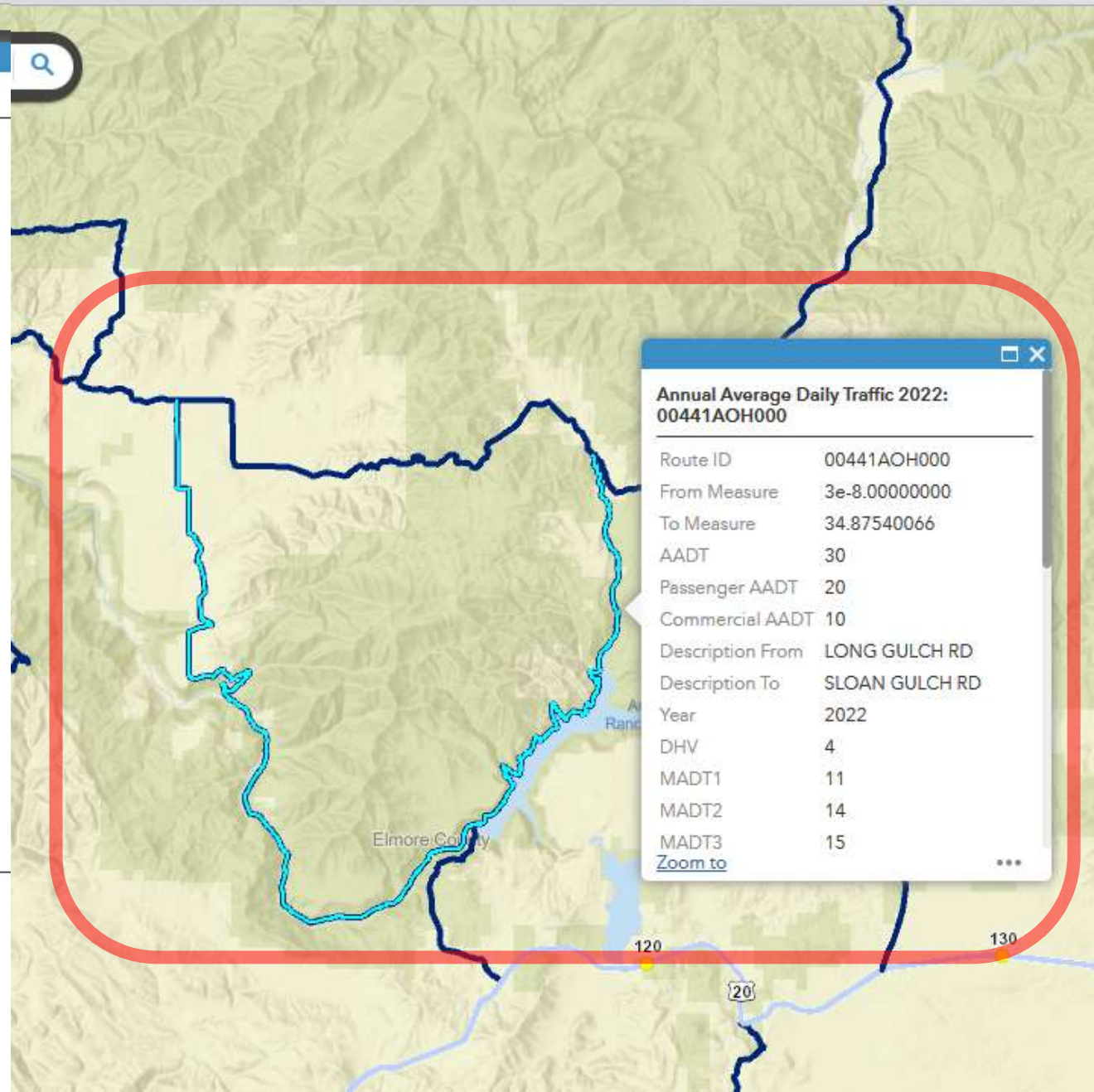
Monthly Averages
December Low = 9/day
July High = 67/day

Route ID: 00441AOH000

In 2022, this segment had a Total Daily Traffic of 30, with a BM 3e-8.00000000 and EM of 34.87540066.



Zoom to



Annual Average Daily Traffic 2022: 00441AOH000

Route ID	00441AOH000
From Measure	3e-8.00000000
To Measure	34.87540066
AADT	30
Passenger AADT	20
Commercial AADT	10
Description From	LONG GULCH RD
Description To	SLOAN GULCH RD
Year	2022
DHV	4
MADT1	11
MADT2	14
MADT3	15
Zoom to	...

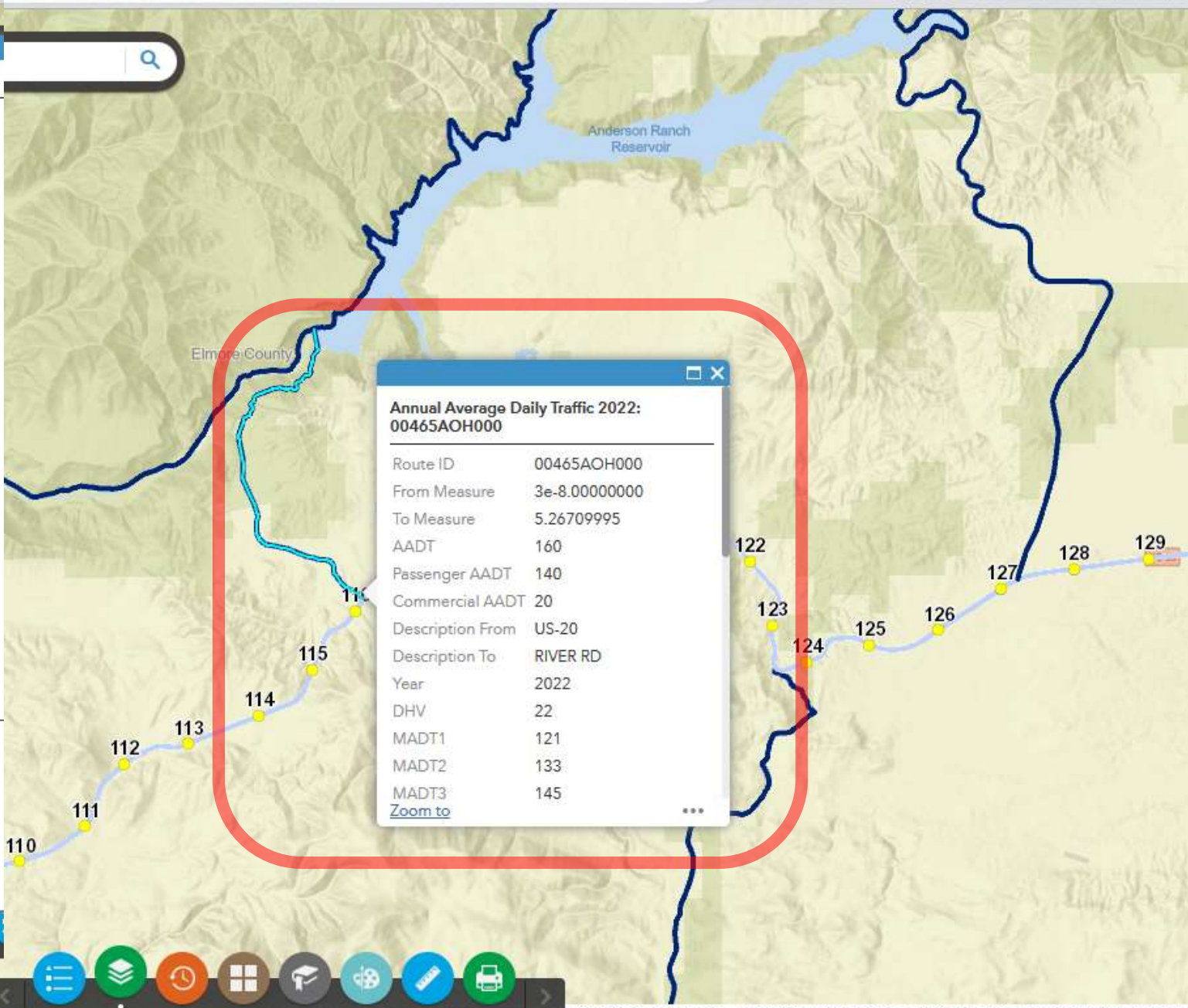
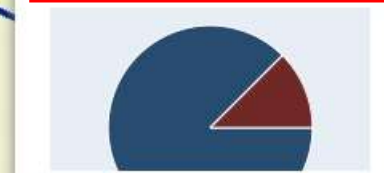
Annual Average Daily Traffic 2022: 00465AOH000

Route ID	00465AOH000
From Measure	3e-8.00000000
To Measure	5.26709995
AADT	160
Passenger AADT	140
Commercial AADT	20
Description From	US-20
Description To	RIVER RD
Year	2022
DHV	22
MADT1	121
MADT2	133
MADT3	145
MADT4	151
MADT5	170
MADT6	186
MADT7	195
MADT8	191
MADT9	183
MADT10	172
MADT11	142
MADT12	119

~1 vehicle / 2.5-3 mins

Monthly Averages
December Low = 119/day
July High = 195/day

Route ID: 00465AOH000
In 2022, this segment had a Total Daily Traffic of 160, with a BM 3e-8.00000000 and EM of 5.26709995.



Annual Average Daily Traffic 2022: 00465AOH000

Route ID	00465AOH000
From Measure	3e-8.00000000
To Measure	5.26709995
AADT	160
Passenger AADT	140
Commercial AADT	20
Description From	US-20
Description To	RIVER RD
Year	2022
DHV	22
MADT1	121
MADT2	133
MADT3	145

Zoom to

Bridges (Conditions, Ratings)

Select and customize an area filter

County

City

Local Road Juris

Legislative District

Bridge Filters

Bridge Statistics

[All bridges in selected area\(s\): 3843](#)
[Local bridges in selected area\(s\): 2479](#)
[Shown on Map \(filtered\): 3843](#)
 Selected: 1
 Highlighted: 0

- Bridges**
- △ State, No restriction (1358)
 - △ State, Status other (0)
 - △ State, Posting recommended (0)
 - △ State, Posted for load (5)
 - △ State, Closed to all traffic (1)
 - △ Local, No restriction (2120)
 - △ Local, Status other (0)
 - △ Local, Posting recommended (6)
 - △ Local, Posted for load (350)
 - △ Local, Closed to all traffic (3)

2022 Idaho Bridge Data



Bridge Details - click to highlight in map

Bridge Key	Highway System	Structure Name	Year Built	Year Reconstructed	Carries	Crosses Over	Length (Ft)	Deck Width	Post Status	Condition	Lanes On	ADT	ADT Year	Culvert	County	Admin Juris	Deck Str Type	Deck Surf Type	Historical Significance	Date Received
27840	local	X993200 7.62	1961		NEAL ROAD	S.FK.BOISE RIVER	128	16.1	A Open, no restriction	Fair	1	90	2019	N	Elmore	Mountain Home HD	8 Wood or Timber	6 Bituminous	4 Hist sign not determin	2022-03-29

Select and customize an area filter

County

City

Local Road Juris

Legislative District

Bridge Filters

Bridge Statistics

[All bridges in selected area\(s\): 3843](#)
[Local bridges in selected area\(s\): 2479](#)
[Shown on Map \(filtered\): 3843](#)
 Selected: 1
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 - △ State, Closed to all traffic (1)
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 - △ Local, Posting recommended (6)
 - △ Local, Posted for load (350)
 - △ Local, Closed to all traffic (3)

2022 Idaho Bridge Data



Bridge Details - click to highlight in map

Bridge Key	Highway System	Structure Name	Year Built	Year Reconstructed	Carries	Crosses Over	Length (Ft)	Deck Width	Post Status	Condition	Lanes On	ADT	ADT Year	Culvert	County	Admin Juris	Deck Str Type	Deck Surf Type	Historical Significance	Date Received
27858	local	X993200 110.32	2012		NFD RD113;PRAIRIE	PIERCE CREEK	31	27.1	A Open, no restriction	Good	2	30	2019	N	Elmore	Mountain Home HD	6 Corrugated Steel	8 Gravel	4 Hist sign not determin	2022-03-29

Select and customize an area filter

County

City

Local Road Juris

Legislative District

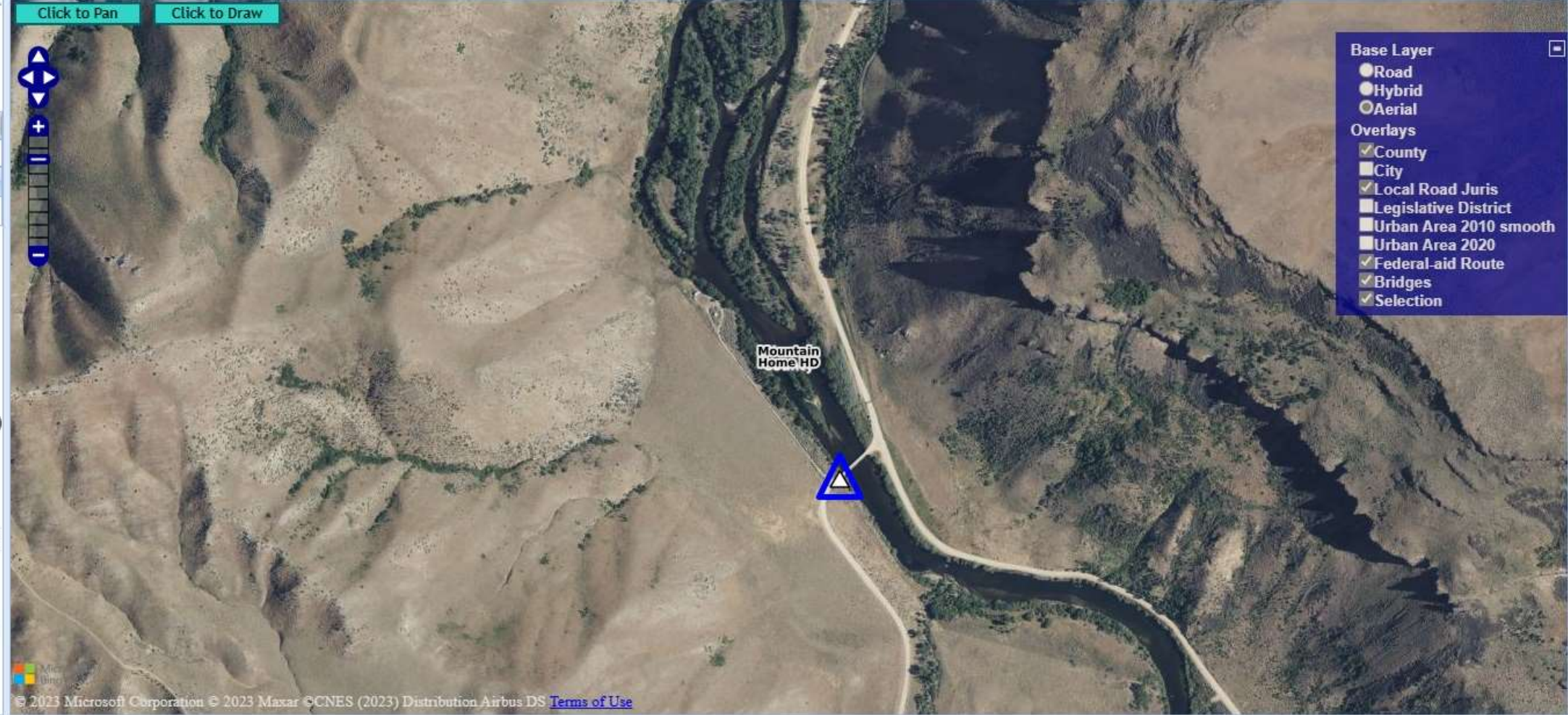
Bridge Filters

Bridge Statistics

[All bridges in selected area\(s\): 3843](#)
[Local bridges in selected area\(s\): 2479](#)
[Shown on Map \(filtered\): 3843](#)
 Selected: 1
 Highlighted: 0

- Bridges**
- △ State, No restriction (1358)
 - △ State, Status other (0)
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2022 Idaho Bridge Data

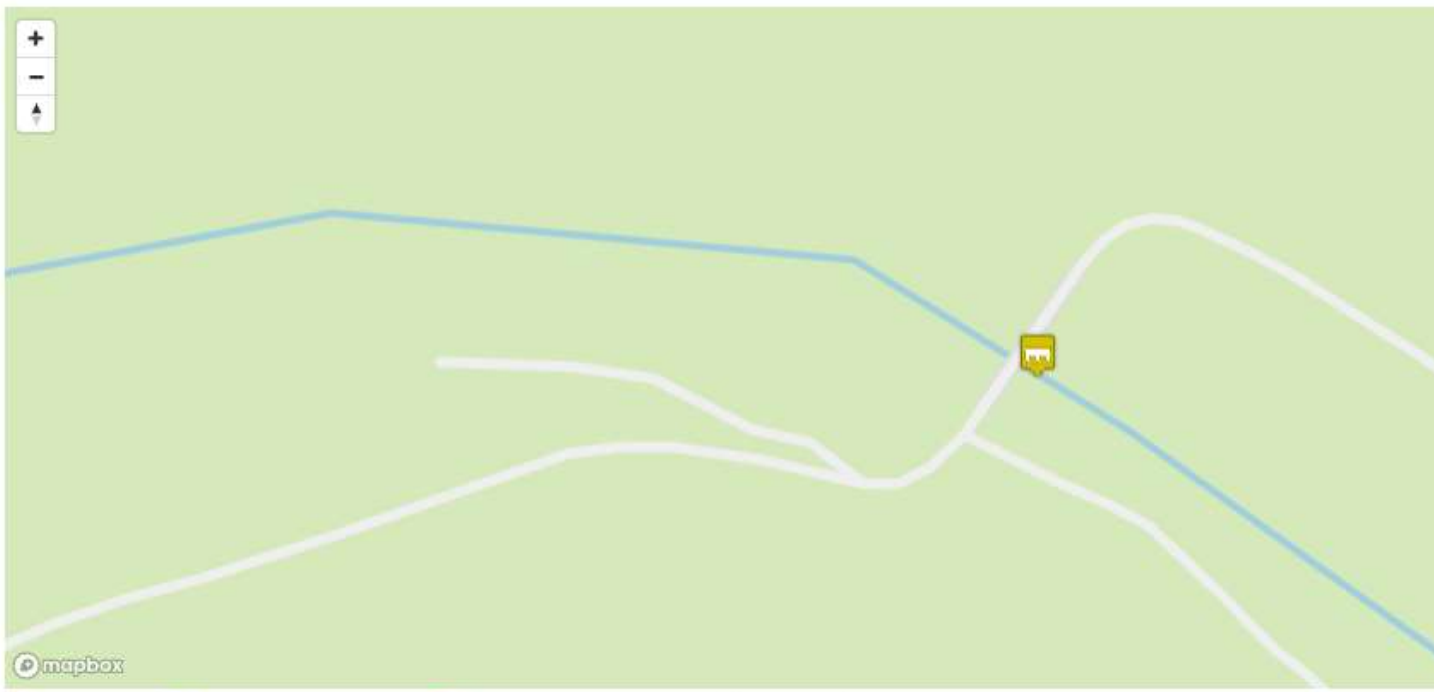


Bridge Details - click to highlight in map

Bridge Key	Highway System	Structure Name	Year Built	Year Reconstructed	Carries	Crosses Over	Length (Ft)	Deck Width	Post Status	Condition	Lanes On	ADT	ADT Year	Culvert	County	Admin Juris	Deck Str Type	Deck Surf Type	Historical Significance	Date Received
27855	local	X993200 10.36	1959		NFD 131 RD	S.FK.BOISE R. (COW CR.BR)	154	16	A Open, no restriction	Fair	1	20	2019	N	Elmore	Mountain Home HD	1 Concrete-Cast-in-Place	1 Monolithic Concrete	4 Hist sign not determin	2022-03-29

Bridge Inspections
DANSKIN ROAD over S. FK. BOISE RIVER

Map



■ Good condition
 ■ Meets minimum tolerable limits
 ■ Needs repair or corrective action
 ■ Closed
 ■ Report not available

Basic Information

County, State:	Elmore County, Idaho	Structure Number:	040201000001202
Maintenance Responsibility:	U.S. Forest Service	Ownership:	U.S. Forest Service
Facility Carried By Structure:	DANSKIN ROAD	Features Intersected (Location):	S. FK. BOISE RIVER (7 MI NW ANDRSN RNCH DAM)
Year Built:	1957	Year Reconstructed:	N/A
Average Daily Traffic (Year):	10 (1992)	Future Average Daily Traffic (Year):	15 (2043)

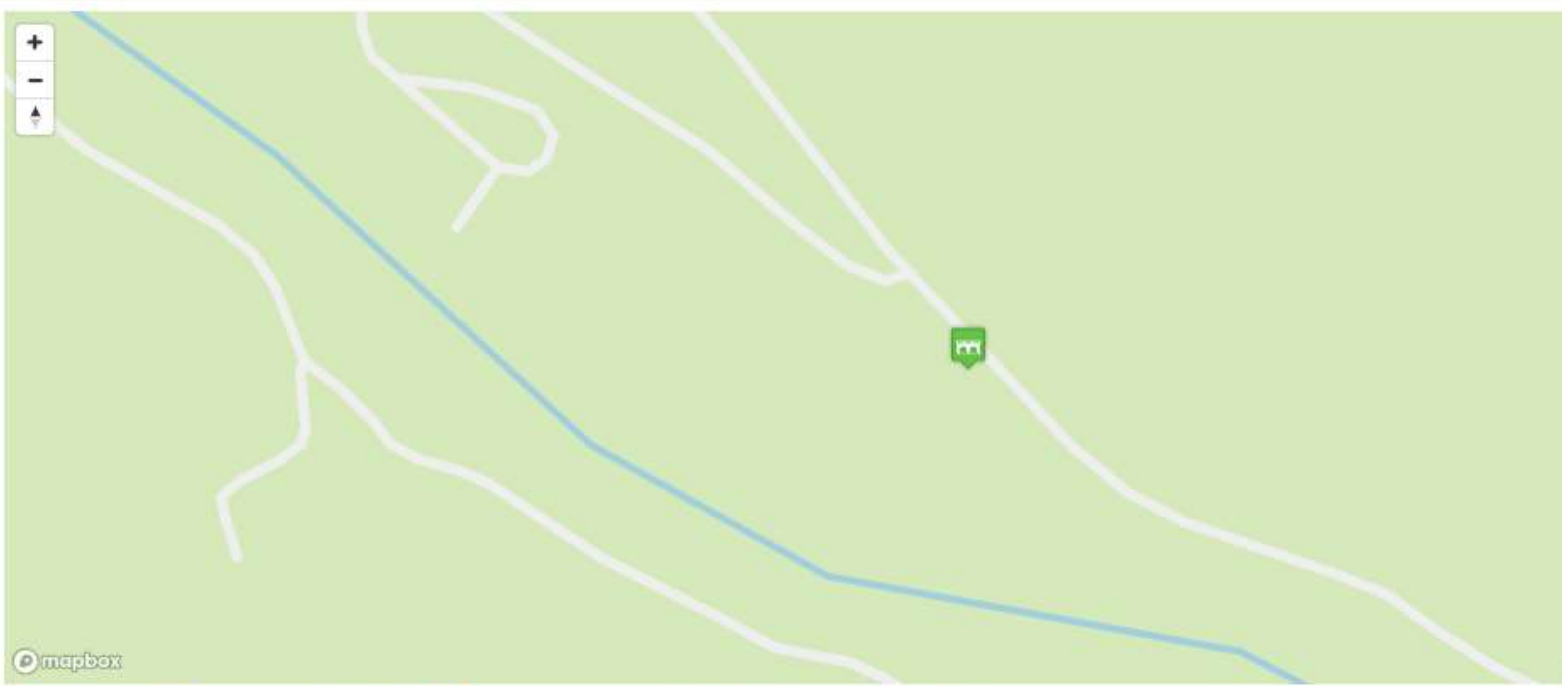
NBI Report

NBI Bridge Condition	Fair
Inspection Date (Inspection Frequency):	July 2021 (Every 24 Months)
Structural Evaluation:	4: Meets minimum tolerable limits to be left in place as is
Deck Geometry	4: Meets minimum tolerable limits to be left in place as is
Underclear/Vertical and Horizontal:	N: Not applicable
Deck	5: FAIR CONDITION - all primary structural elements are sound but may have minor section loss, cracking, spalling or scour.
Superstructure	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Substructure	7: GOOD CONDITION - some minor problems.
Channel/Channel Protection	7: Bank protection is in need of minor repairs. River control devices and embankment protection have a little minor damage. Banks and/or channel have minor amounts of drift.
Culverts	N: Not applicable. Use if structure is not a culvert.
Waterway Adequacy:	9: Bridge deck and roadway approaches above flood water elevations (high water). Chance of overtopping is remote.
Approach Roadway Alignment:	6: Equal to present minimum criteria
Bridge Posting	No posting required: Equal to or above legal loads
Pier/Abutment Protection:	1: Navigation protection not required
Scour Critical Bridges:	5: Bridge foundations determined to be stable for calculated scour conditions; scour within limits of footing or piles.
Bridge Railings:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Transitions:	N: Not applicable
Approach Guardrail:	N: Not applicable
Approach Guardrail Ends:	N: Not applicable
Historical significance:	4: Historical significance is not determinable at this time.
Fracture Critical Details:	Y24

NBI Report

Bridge Inspections
NFD RD113;PRAIRIE over PIERCE CREEK

Map



NBI Bridge Condition	Good
Inspection Date (Inspection Frequency):	June 2018 (Every 48 Months)
Structural Evaluation:	8: Equal to present desirable criteria
Deck Geometry	6: Equal to present minimum criteria
Underclear/Vertical and Horizontal:	N: Not applicable
Deck	8: VERY GOOD CONDITION - no problems noted.
Superstructure	8: VERY GOOD CONDITION - no problems noted.
Substructure	8: VERY GOOD CONDITION - no problems noted.
Channel/Channel Protection	6: Bank is beginning to slump. River control devices and embankment protection have widespread minor damage. There is minor stream bed movement evident. Debris is restricting the channel slightly.
Culverts	N: Not applicable. Use if structure is not a culvert.
Waterway Adequacy:	7: Slight chance of overtopping bridge deck and roadway approaches.
Approach Roadway Alignment:	8: Equal to present desirable criteria
Bridge Posting	No posting required: Equal to or above legal loads
Scour Critical Bridges:	8: Bridge foundations determined to be stable for assessed or calculated scour conditions; calculated scour is above top of footing.
Bridge Railings:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Transitions:	1: Inspected feature meets currently acceptable standards.
Approach Guardrail:	1: Inspected feature meets currently acceptable standards.
Approach Guardrail Ends:	1: Inspected feature meets currently acceptable standards.
Historical significance:	4: Historical significance is not determinable at this time.

Basic Information

County, State:	Elmore County, Idaho	Structure Number:	00000000027858
Maintenance Responsibility:	Other Local Agencies	Ownership:	Other Local Agencies
Facility Carried By Structure:	NFD RD113;PRAIRIE	Features Intersected (Location):	PIERCE CREEK (7 S 0.8 E PRAIRIE)
Year Built:	2012	Year Reconstructed:	N/A
Average Daily Traffic (Year):	30 (2019) with 33% of truck traffic	Future Average Daily Traffic (Year):	45 (2039)

Bridge Inspections
NFD 131 RD over S.FK.BOISE R.(COW CR.BR)

Map



■ Good condition
 ■ Meets minimum tolerable limits
 ■ Needs repair or corrective action
 ■ Closed
 ■ Report not available

Basic Information

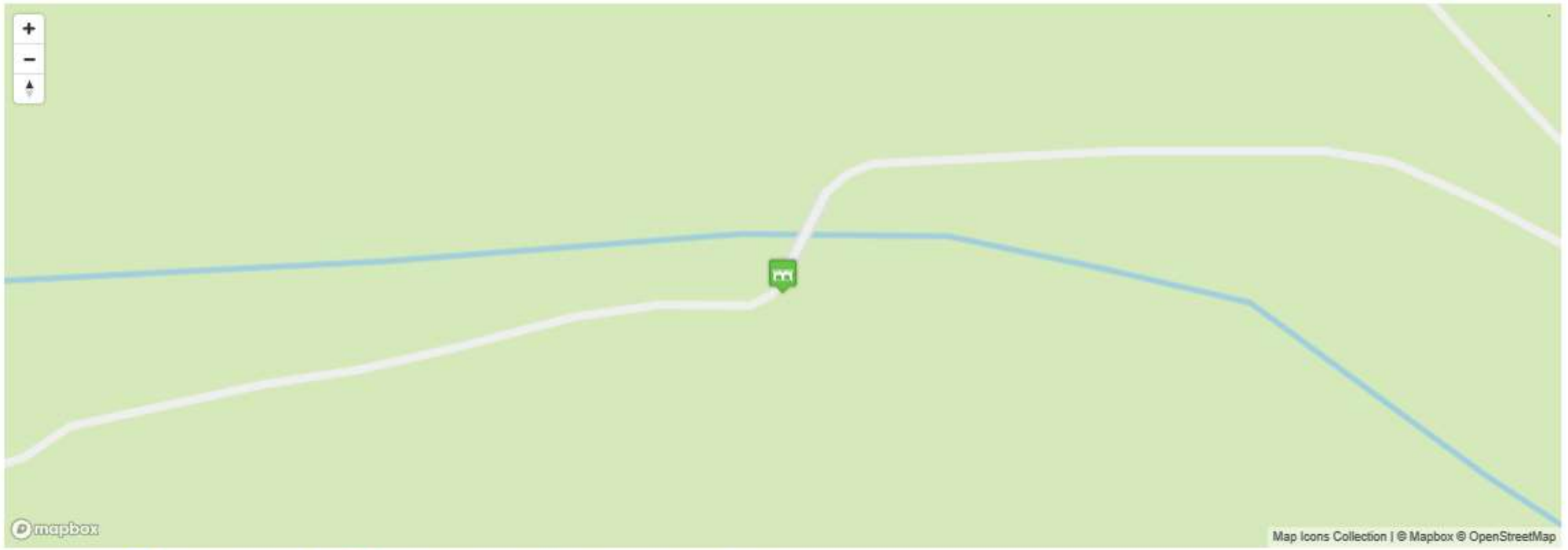
County, State:	Elmore County, Idaho	Structure Number:	00000000027855
Maintenance Responsibility:	Other Local Agencies	Ownership:	Other Local Agencies
Facility Carried By Structure:	NFD 131 RD	Features Intersected (Location):	S.FK.BOISE R.(COW CR.Br) (S.S. T.O.E. PRAIRIE)
Year Built:	1959	Year Reconstructed:	N/A
Average Daily Traffic (Year):	20 (2019) with 10% of truck traffic	Future Average Daily Traffic (Year):	30 (2039)

NBI Report

NBI Bridge Condition	Fair
Inspection Date (Inspection Frequency):	June 2022 (Every 24 Months)
Structural Evaluation:	6: Equal to present minimum criteria
Deck Geometry	6: Equal to present minimum criteria
Underclear/Vertical and Horizontal:	N: Not applicable
Deck	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Superstructure	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Substructure	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Channel/Channel Protection	7: Bank protection is in need of minor repairs. River control devices and embankment protection have a little minor damage. Banks and/or channel have minor amounts of drift.
Culverts	N: Not applicable. Use if structure is not a culvert.
Waterway Adequacy:	6: Bridge deck above roadway approaches. Occasional overtopping of roadway approaches with insignificant traffic delays.
Approach Roadway Alignment:	5: Somewhat better than minimum adequacy to tolerate being left in place as is
Bridge Posting	No posting required: Equal to or above legal loads
Scour Critical Bridges:	5: Bridge foundations determined to be stable for calculated scour conditions; scour within limits of footing or piles.
Bridge Railings:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Transitions:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Approach Guardrail:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Approach Guardrail Ends:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Historical significance:	4: Historical significance is not determinable at this time.

Bridge Inspections NEAL ROAD over S.FK.BOISE RIVER

Map



■ Good condition
 ■ Meets minimum tolerable limits
 ■ Needs repair or corrective action
 ■ Closed
 ■ Report not available

Basic Information

County, State:	Elmore County, Idaho	Structure Number:	00000000027840
Maintenance Responsibility:	Other Local Agencies	Ownership:	Other Local Agencies
Facility Carried By Structure:	NEAL ROAD	Features Intersected (Location):	S.FK.BOISE RIVER (3.1 N. 7.4 W. PRAIRIE)
Year Built:	1961	Year Reconstructed:	N/A
Average Daily Traffic (Year):	90 (2019) with 11% of truck traffic	Future Average Daily Traffic (Year):	135 (2039)

Map



Basic Information

County, State:	
Maintenance Responsibility:	
Facility Carried By Structure:	
Year Built:	
Average Daily Traffic (Year):	

NBI Bridge Condition	Fair
Inspection Date (Inspection Frequency):	Sept. 2021 (Every 24 Months)
Structural Evaluation:	6: Equal to present minimum criteria
Deck Geometry	5: Somewhat better than minimum adequacy to tolerate being left in place as is
Underclear/Vertical and Horizontal:	N: Not applicable
Deck	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Superstructure	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Substructure	6: SATISFACTORY CONDITION - structural elements show some minor deterioration.
Channel/Channel Protection	7: Bank protection is in need of minor repairs. River control devices and embankment protection have a little minor damage. Banks and/or channel have minor amounts of drift.
Culverts	N: Not applicable. Use if structure is not a culvert.
Waterway Adequacy:	9: Bridge deck and roadway approaches above flood water elevations (high water). Chance of overtopping is remote.
Approach Roadway Alignment:	6: Equal to present minimum criteria
Bridge Posting	No posting required: Equal to or above legal loads
Scour Critical Bridges:	3: Bridge is scour critical; bridge foundations determined to be unstable for calculated scour conditions: 1) Scour within limits of footing or piles. 2) Scour below spread-footing base or pile tips.
Bridge Railings:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Transitions:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Approach Guardrail:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Approach Guardrail Ends:	0: Inspected feature does not meet currently acceptable standards or a safety feature is required and none is provided.
Historical significance:	4: Historical significance is not determinable at this time.
Type of Work Proposed:	Replacement of bridge or other structure because of substandard load carrying capacity or substandard bridge roadway geometry.
Work Done By:	Work to be done by contract
Length of Structure Improvement:	4.85 meters
Bridge Improvement Cost	\$787,000
Roadway Improvement Cost	\$79,000
Total Project Cost	\$1,181,000
Year of Improvement Cost Estimate:	2021
Fracture Critical Details:	Y24